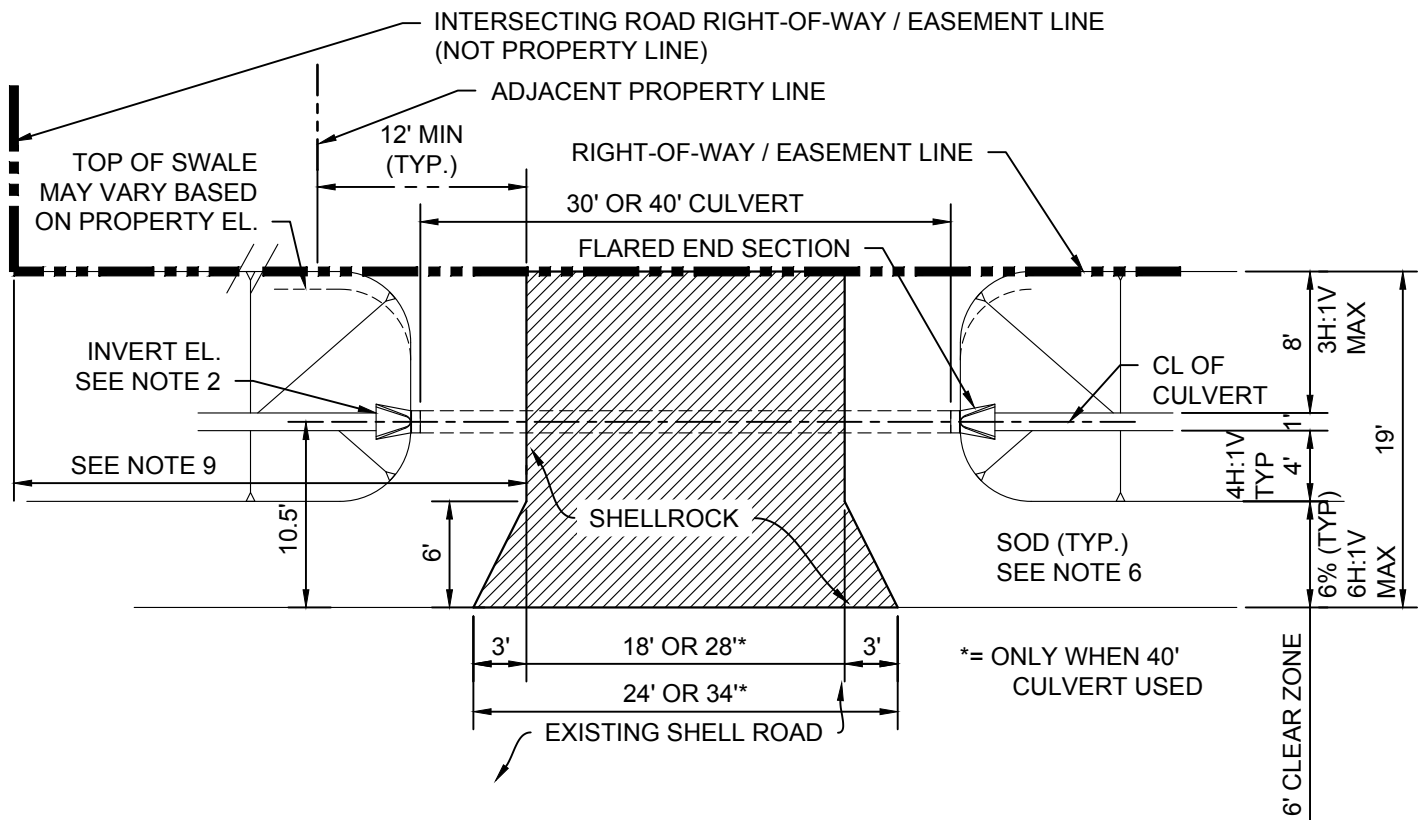


# ATTACHMENT B.1



## INDIAN TRAIL IMPROVEMENT DISTRICT 60' EASEMENT DRIVEWAY CONNECTION/CULVERT DETAIL (SHELLROCK ROAD)

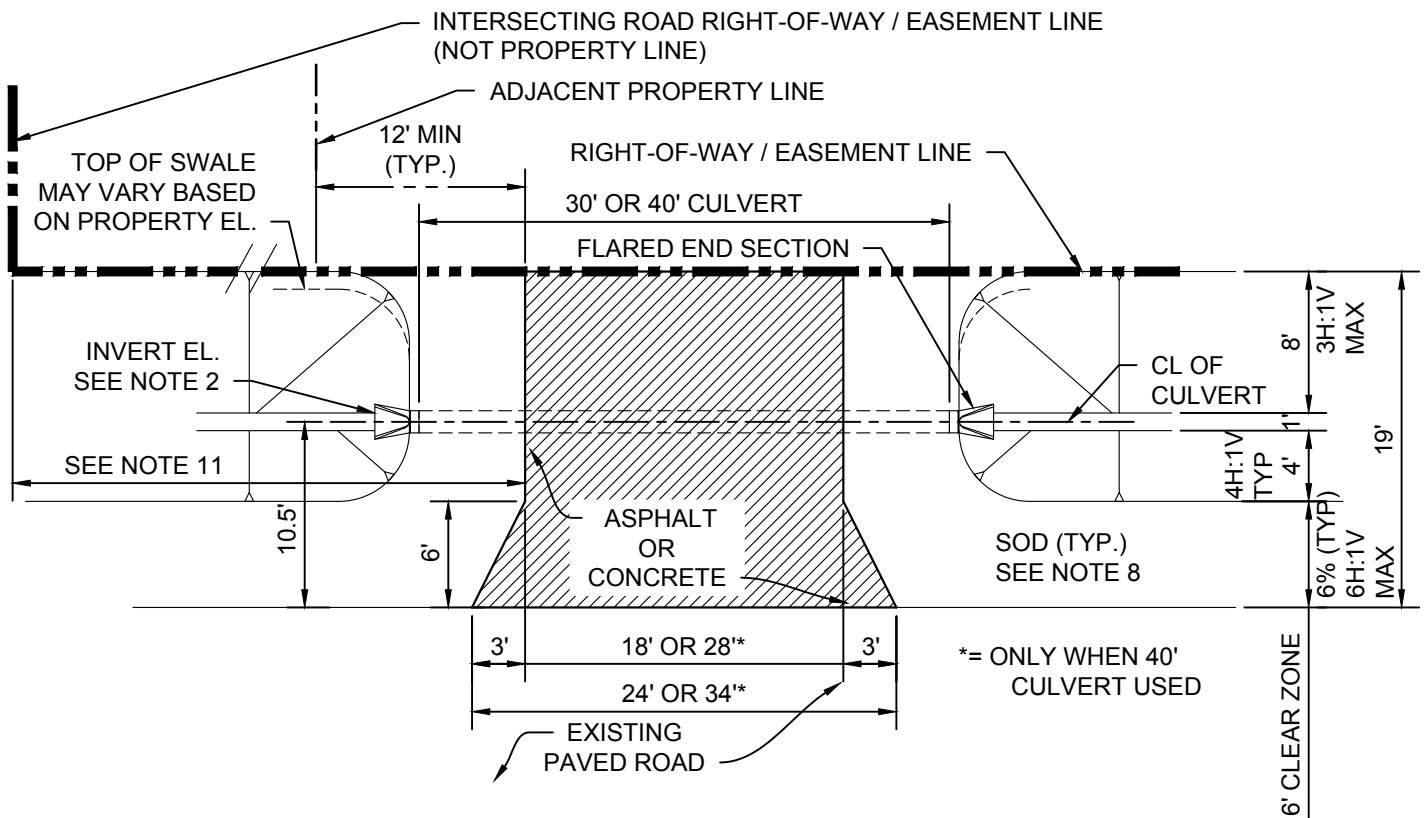
N.T.S.

### NOTES:

1. CULVERT MATERIAL, DIAMETER AND FLARED END SECTIONS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
2. ALL PIPE LOCATIONS AND ELEVATIONS SHALL BE SET BY THE DISTRICT.
3. SHELLROCK SHALL BE MIN. 6" IN THICKNESS AND COMPACTED TO MIN. 98% MAX. DRY DENSITY PER AASHTO T-180. THE MINIMUM COVER OF SHELLROCK OVER THE CULVERT SHALL BE 6".
4. ALL BACKFILL AND BASE SHALL BE COMPACTED TO THE ABOVE SPECIFICATIONS.
5. DRIVEWAY ELEVATION SHALL MATCH ROADWAY ELEVATION AT CONNECTION POINT.
6. SWALES SHALL BE GRADED AND SODDED WITH BAHIA ALONG THE ENTIRE FRONTAGE OF THE PROPERTY IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
7. UNLESS NEEDED OTHERWISE, ALL ROADSIDE SWALE SLOPES SHALL BE 4H:1V OR FLATTER.
8. SHELLROCK DRIVEWAY SHALL BE CONSTRUCTED FROM EDGE OF EXISTING ROADWAY TO THE RIGHT-OF-WAY/EASEMENT LINE ON BOTH SIDEWALK AND NON-SIDEWALK SIDES.
9. THE EDGE OF DRIVEWAY SHALL BE SET BACK A MINIMUM FROM THE RIGHTS-OF-WAY/EASEMENT LINES FROM ADJACENT INTERSECTING ROADWAYS AS FOLLOWS:
  - MIN. 30' TO DISTRICT NEIGHBORHOOD ROADS
  - MIN. 50' TO DISTRICT MAJOR OR PALM BEACH COUNTY COLLECTOR ROADS
  - MIN. 75' TO PALM BEACH COUNTY ARTERIAL ROADS

# ATTACHMENT B.2

ADOPTED : 5/15/19 ITID BOS



## INDIAN TRAIL IMPROVEMENT DISTRICT

### 60' EASEMENT DRIVEWAY CONNECTION/CULVERT DETAIL (PAVED ROAD)

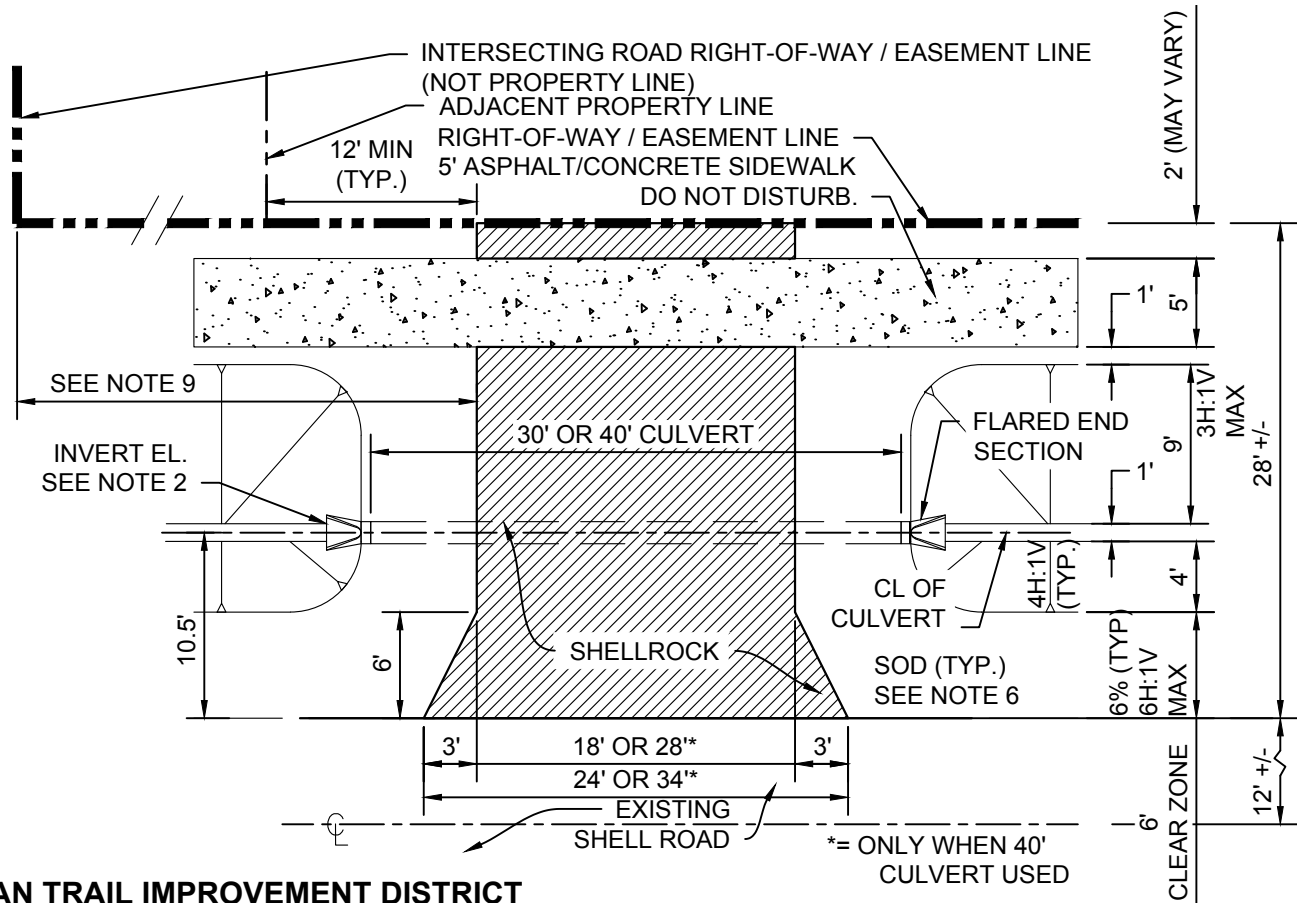
N.T.S.

#### NOTES:

1. CULVERT MATERIAL, DIAMETER AND FLARED END SECTIONS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
2. ALL PIPE LOCATIONS AND ELEVATIONS SHALL BE SET BY THE DISTRICT.
3. ALL BACKFILL AND BASE SHALL BE COMPACTED TO THE ABOVE SPECIFICATIONS.
4. ASPHALT SHALL BE 1-1/2" TYPE S-3, OVER A MIN. 8" SHELLROCK BASE, COMPACTED TO MIN. 98% MAX. DRY DENSITY PER AASHTO T-180. EXTEND BASE MIN. 3" BEYOND EDGE OF ASPHALT. CONCRETE SHALL BE MIN. 6" THICK, 4,000 PSI, OVER MIN 4" COMPACTED SAND BASE. THE MINIMUM COVER OVER THE CULVERT SHALL BE 8".
5. REFER TO PALM BEACH COUNTY DESIGN STANDARDS MANUAL SECTION 300 FOR THICKENED EDGE END DETAIL AND SAW CUT REQUIREMENTS OF CONCRETE DRIVEWAYS.
6. PRIOR TO INSTALLATION OF ASPHALT/CONCRETE, CONTACT THE DISTRICT TO SCHEDULE AN INSPECTION OF LAYOUT AND FORMWORK.
7. DRIVEWAY ELEVATION SHALL MATCH ROADWAY ELEVATION AT CONNECTION POINT.
8. SWALES SHALL BE GRADED AND SODDED WITH BAHIA ALONG THE ENTIRE FRONTAGE OF THE PROPERTY IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
9. UNLESS NEEDED OTHERWISE, ALL ROADSIDE SWALE SLOPES SHALL BE 4H:1V OR FLATTER.
10. SHELLROCK/ASPHALT/CONCRETE DRIVEWAY SHALL BE CONSTRUCTED FROM EDGE OF EXISTING ROADWAY TO THE RIGHT-OF-WAY/EASEMENT LINE ON BOTH SIDEWALK AND NON-SIDEWALK SIDES.
11. THE EDGE OF DRIVEWAY SHALL BE SET BACK A MINIMUM FROM THE RIGHTS-OF-WAY/EASEMENT LINES FROM ADJACENT INTERSECTING ROADWAYS AS FOLLOWS:
  - MIN. 30' TO DISTRICT NEIGHBORHOOD ROADS
  - MIN. 50' TO DISTRICT MAJOR OR PALM BEACH COUNTY COLLECTOR ROADS
  - MIN. 75' TO PALM BEACH COUNTY ARTERIAL ROADS

# ATTACHMENT B.3

ADOPTED: 5/15/19 ITID BOS



## INDIAN TRAIL IMPROVEMENT DISTRICT

### 80' EASEMENT DRIVEWAY CONNECTION/CULVERT DETAIL (SHELLROCK ROAD)

(WITH OR WITHOUT SIDEWALK)

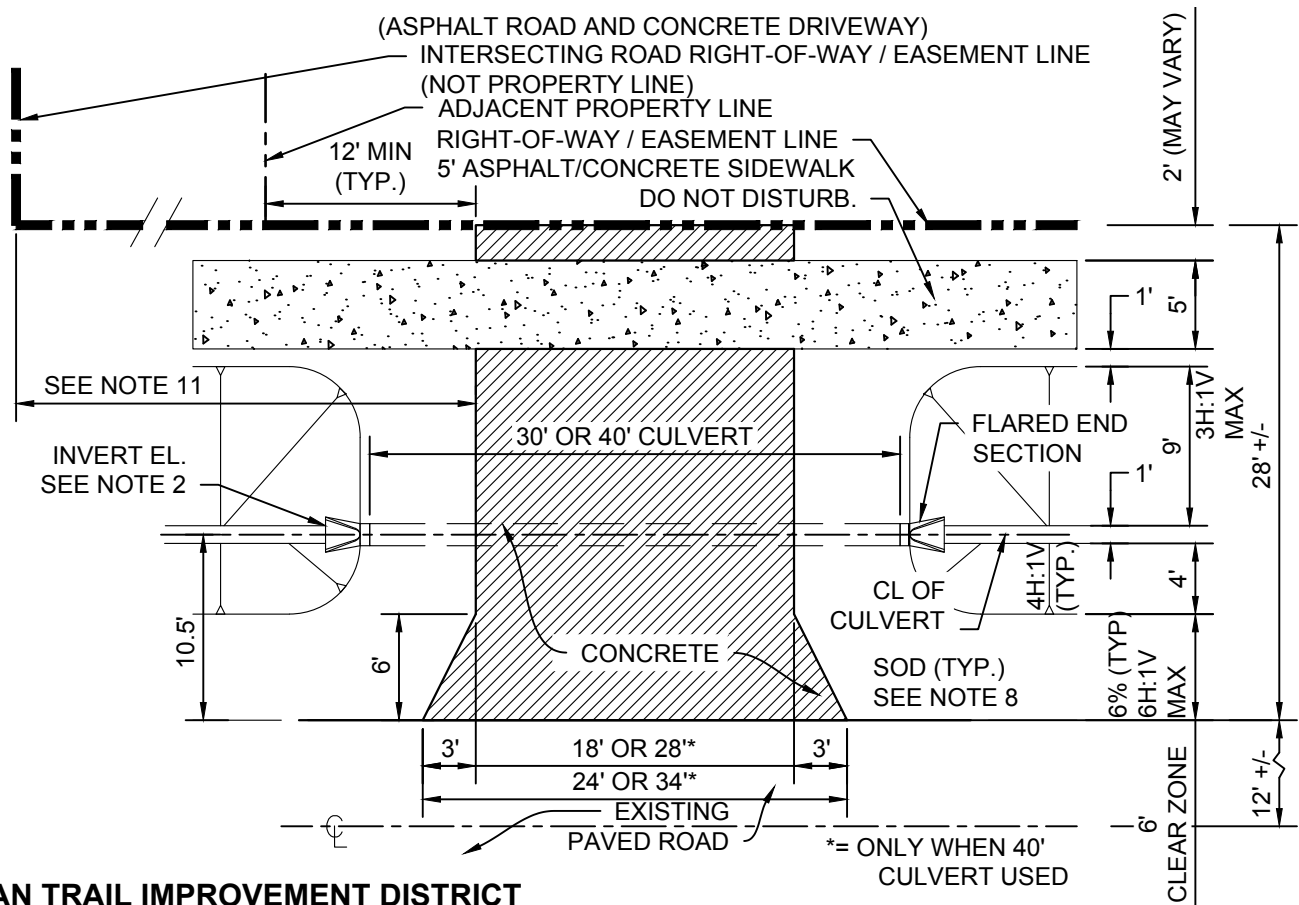
N.T.S.

#### NOTES:

1. CULVERT MATERIAL, DIAMETER AND FLARED END SECTIONS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
2. ALL PIPE LOCATIONS AND ELEVATIONS SHALL BE SET BY THE DISTRICT.
3. SHELLROCK SHALL BE MIN. 6" IN THICKNESS AND COMPACTED TO MIN. 98% MAX. DRY DENSITY PER AASHTO T-180. THE MINIMUM COVER OF SHELLROCK OVER THE CULVERT SHALL BE 6".
4. ALL BACKFILL AND BASE SHALL BE COMPACTED TO THE ABOVE SPECIFICATIONS.
5. DRIVEWAY SHALL MATCH ROADWAY AND SIDEWALK ELEVATIONS AT CONNECTION POINTS.
6. SWALES SHALL BE GRADED AND SODDED WITH BAHIA ALONG THE ENTIRE FRONTAGE OF THE PROPERTY IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
7. UNLESS NEEDED OTHERWISE, ALL ROADSIDE SWALE SLOPES SHALL BE 4H:1V OR FLATTER.
8. SHELLROCK DRIVEWAY SHALL BE CONSTRUCTED FROM EDGE OF EXISTING ROADWAY TO THE RIGHT-OF-WAY/EASEMENT LINE ON BOTH SIDEWALK AND NON-SIDEWALK SIDES.
9. THE EDGE OF DRIVEWAY SHALL BE SET BACK A MINIMUM FROM THE RIGHTS-OF-WAY/EASEMENT LINES FROM ADJACENT INTERSECTING ROADWAYS AS FOLLOWS:
  - MIN. 30' TO DISTRICT NEIGHBORHOOD ROADS
  - MIN. 50' TO DISTRICT MAJOR OR PALM BEACH COUNTY COLLECTOR ROADS
  - MIN. 75' TO PALM BEACH COUNTY ARTERIAL ROADS.
10. R-1, R-2, MINOR, AND MAJOR ROADS HAVE SEPARATE ROAD SECTIONS AND THE ABOVE MUST BE MODIFIED TO MATCH THESE ROAD SECTIONS.

# ATTACHMENT B.4

ADOPTED: 5/15/19 ITID BOS



## **INDIAN TRAIL IMPROVEMENT DISTRICT 80' EASEMENT DRIVEWAY CONNECTION/CULVERT DETAIL (PAVED ROAD)**

(WITH OR WITHOUT SIDEWALK)

N.T.S.

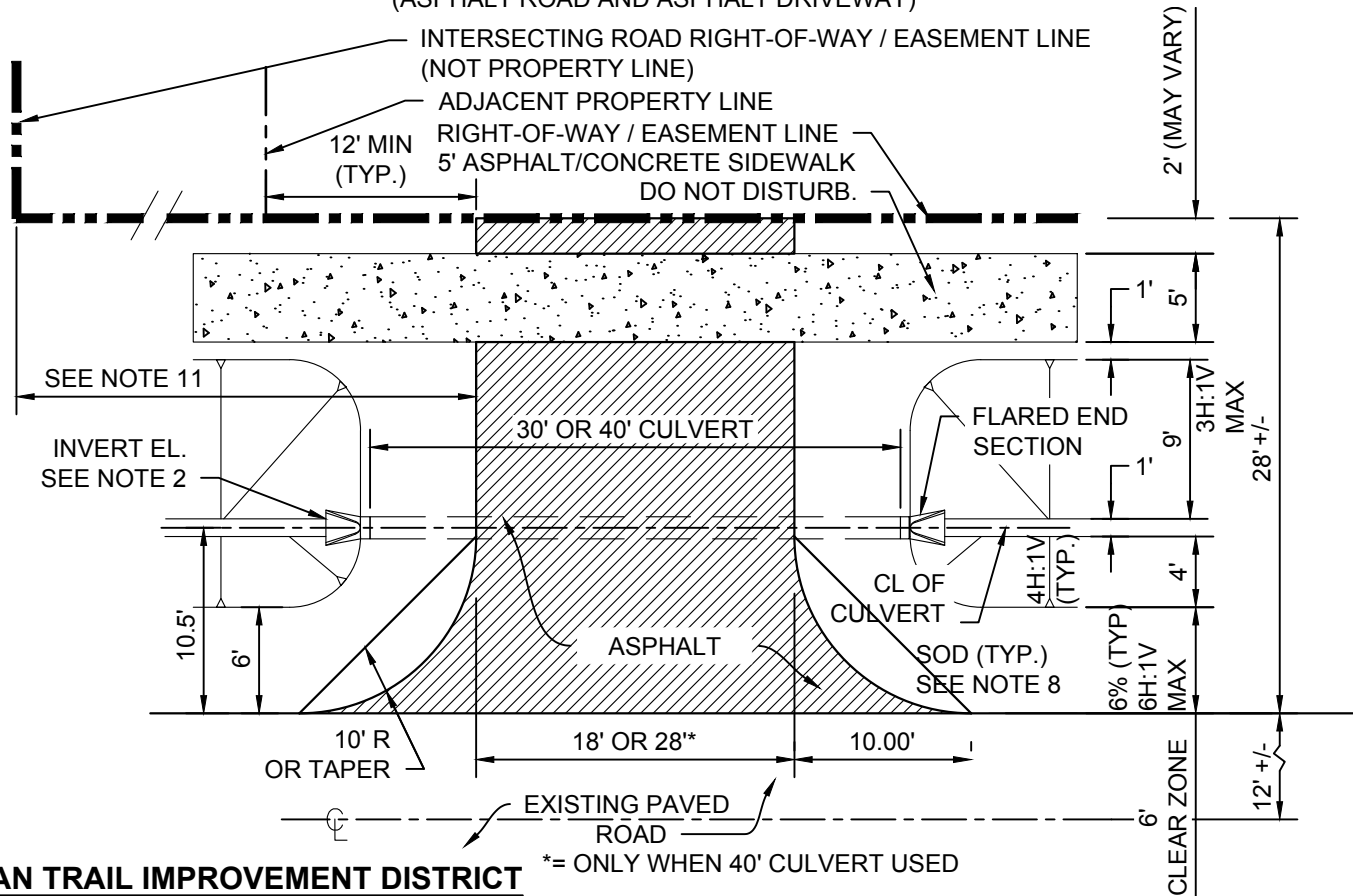
### NOTES:

1. CULVERT MATERIAL, DIAMETER AND FLARED END SECTIONS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
2. ALL PIPE LOCATIONS AND ELEVATIONS SHALL BE SET BY THE DISTRICT.
3. ALL BACKFILL AND BASE SHALL BE COMPACTED TO THE ABOVE SPECIFICATIONS.
4. CONCRETE SHALL BE MIN 6" THICK, 4,000 PSI OVER MIN. 4" COMPACTED SAND BASE. THE MINIMUM COVER OVER THE CULVERT SHALL BE 8".
5. REFER TO PALM BEACH COUNTY DESIGN STANDARDS MANUAL SECTION 300 FOR THICKENED EDGE END DETAIL AND SAW CUT REQUIREMENTS OF CONCRETE DRIVEWAYS.
6. PRIOR TO INSTALLATION OF THE CONCRETE, CONTACT THE DISTRICT TO SCHEDULE AN INSPECTION OF LAYOUT AND FORMWORK.
7. DRIVEWAY SHALL MATCH ROADWAY AND SIDEWALK ELEVATIONS AT CONNECTION POINTS.
8. SWALES SHALL BE GRADED AND SODDED WITH BAHIA ALONG THE ENTIRE FRONTAGE OF THE PROPERTY IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
9. UNLESS NEEDED OTHERWISE, ALL ROADSIDE SWALE SLOPES SHALL BE 4H:1V OR FLATTER.
10. CONCRETE DRIVEWAY SHALL BE CONSTRUCTED FROM EDGE OF EXISTING ROADWAY TO THE RIGHT-OF-WAY/EASEMENT LINE ON BOTH SIDEWALK AND NON-SIDEWALK SIDES.
11. THE EDGE OF DRIVEWAY SHALL BE SET BACK A MINIMUM FROM THE RIGHTS-OF-WAY/EASEMENT LINES FROM ADJACENT INTERSECTING ROADWAYS AS FOLLOWS:
  - MIN. 30' TO DISTRICT NEIGHBORHOOD ROADS
  - MIN. 50' TO DISTRICT MAJOR OR PALM BEACH COUNTY COLLECTOR ROADS
  - MIN. 75' TO PALM BEACH COUNTY ARTERIAL ROADS.
12. R-1, R-2, MINOR, AND MAJOR ROADS HAVE SEPARATE ROAD SECTIONS AND THE ABOVE MUST BE MODIFIED TO MATCH THESE ROAD SECTIONS.
13. SEE B.4 ALTERNATE FOR ASPHALT DRIVEWAYS AND ASPHALT ROADS.

# ATTACHMENT B.4 - ALTERNATE

ADOPTED: 5/15/19 ITID BOS

(ASPHALT ROAD AND ASPHALT DRIVEWAY)



## INDIAN TRAIL IMPROVEMENT DISTRICT

### 80' EASEMENT DRIVEWAY CONNECTION/CULVERT DETAIL (PAVED ROAD)

(WITH OR WITHOUT SIDEWALK)

N.T.S.

#### NOTES:

1. CULVERT MATERIAL, DIAMETER AND FLARED END SECTIONS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
2. ALL PIPE LOCATIONS AND ELEVATIONS SHALL BE SET BY THE DISTRICT.
3. ALL BACKFILL AND BASE SHALL BE COMPACTED TO THE ABOVE SPECIFICATIONS.
4. ASPHALT SHALL BE 1-1/2" TYPE S-3, OVER A MIN. 8" SHELLROCK BASE, COMPACTED TO MIN. 98% MAX. DRY DENSITY PER AASHTO T-180. EXTEND BASE MIN. 3" BEYOND EDGE OF ASPHALT.
5. DRIVEWAY LAYOUT IS SHOWN FOR ASPHALT INSTALLATION.
6. PRIOR TO INSTALLATION OF ASPHALT, CONTACT THE DISTRICT TO SCHEDULE AN INSPECTION OF LAYOUT AND FORMWORK.
7. DRIVEWAY SHALL MATCH ROADWAY AND SIDEWALK ELEVATIONS AT CONNECTION POINTS.
8. SWALES SHALL BE GRADED AND SODDED WITH BAHIA ALONG THE ENTIRE FRONTAGE OF THE PROPERTY IN ACCORDANCE WITH THE SPECIFICATIONS CONTAINED IN ATTACHMENT A.
9. UNLESS NEEDED OTHERWISE, ALL ROADSIDE SWALE SLOPES SHALL BE 4H:1V OR FLATTER.
10. ASPHALT DRIVEWAY SHALL BE CONSTRUCTED FROM EDGE OF EXISTING ROADWAY TO THE RIGHT-OF-WAY/EASEMENT LINE ON BOTH SIDEWALK AND NON-SIDEWALK SIDES.
11. THE EDGE OF DRIVEWAY SHALL BE SET BACK A MINIMUM FROM THE RIGHTS-OF-WAY/EASEMENT LINES FROM ADJACENT INTERSECTING ROADWAYS AS FOLLOWS:
  - MIN. 30' TO DISTRICT NEIGHBORHOOD ROADS
  - MIN. 50' TO DISTRICT MAJOR OR PALM BEACH COUNTY COLLECTOR ROADS
  - MIN. 75' TO PALM BEACH COUNTY ARTERIAL ROADS.
12. R-1, R-2, MINOR, AND MAJOR ROADS HAVE SEPARATE ROAD SECTIONS AND THE ABOVE MUST BE MODIFIED TO MATCH THESE ROAD SECTIONS.